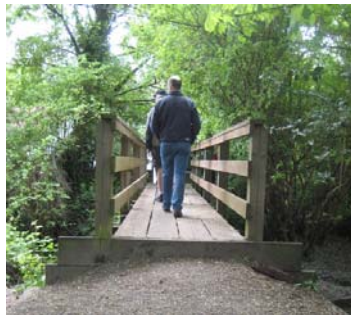


# Trails and Journeyways Strategy

## DISTRICT OF LANTZVILLE

*November 2010*



Prepared by  
**HB Lanarc Consultants Ltd.**



HB Lanarc prepared this document under the direction of the District of Lantzville's Parks and Recreation Commission. We thank the Commission members and District staff for their guidance and commitment to trails and journeyways in Lantzville. We also thank Lantzville residents for sharing their knowledge and ideas in developing this Strategy.

All photos courtesy of Lynne Wilson and Harriet Rueggeberg

**District of Lantzville**  
**TRAILS & JOURNEYWAYS STRATEGY**  
November 2010

**Executive Summary**

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## Executive Summary

The purpose of the Lantzville Trails and Journeyways Strategy is to identify a workable trails and journeyways network in the District of Lantzville and the projects required to put it into place over the short to long term. *Journeyways* are roads used frequently by pedestrians, cyclists and other forms of nonvehicular transportation. *Trails* are off-road paths used to connect between neighbourhoods and to destinations within and beyond Lantzville.

This Strategy discusses major and local journeyways and trails and preliminary design guidelines for each. It also discusses trails along the beach, in Woodlot #1475 and the Foothills Estates property.

The Strategy is accompanied by:

- a) Table 3 (page 15-19), which lists and describes each trail and journeyway improvement project according to priority (1, 2 or 3), location, improvements proposed and cost considerations.
- b) Map A (at end), which identifies a hierarchy of existing and proposed trails and journeyways and the general location of proposed projects.

It is important to note that **project priorities may change**. Unforeseen opportunities may arise to acquire a trail or complete a trail/journeyway project that is of a lower priority or that may not even be on this project list but that meets the goals of the trails and journeyways strategy. These opportunities should not be foregone.

In general, the District's priorities in developing a trail and journeyway system in Lantzville over the next 10 years are to:

- Plan and develop a trail along *the E&N Rail corridor* in cooperation with the Regional District of Nanaimo, neighbouring municipalities and the Island Corridor Foundation.
- Initially focus on trail/journeyway projects that are “quick and easy”, to show progress and gain public momentum; e.g., developing trails within *undeveloped public rights-of-way (ROWS)*.
- Review *upcoming road improvement projects* for opportunities to meet the goals of this Strategy; e.g., widening and marking road shoulders along Lantzville, Dickinson and Superior Roads to improve pedestrian and bicycle safety.
- Continue to work with the Woodlot Licence Holder to protect, maintain and enhance the *trail system in Woodlot #1475*.
- Always be in a position to identify trail and journeyway priorities when development proposals come forward (e.g., project #30 through the Lantzville Properties).

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# 1. Introduction

The *Parks, Trails and Recreation Plan* (2008) for the District of Lantzville identified the need to develop and implement a trails and journeyways strategy (Table 1). Map 2 of the Plan identifies existing and potential trails and journeyways in Lantzville. The next step is to weave these trails and journeyways into a workable network that supports non-vehicular modes of transportation and provides recreational opportunities, and to set priorities for realizing this network.

## 1.1 Purpose and Objectives

The purpose of this Strategy is to identify a workable trails and journeyways network in the District of Lantzville and the projects required to put it into place over the short to long term.

In keeping with the goals of the Parks, Trails and Recreation Plan, the specific objectives are:

- a) To adapt Map 2 from the Parks, Trails and Recreation Plan to identify a workable hierarchy of trails and journeyways. “Looped” routes within neighbourhoods and throughout the community are emphasized.
- b) To identify general design guidelines for trails and journeyways. The design guidelines are intended to provide for safe and enjoyable use without “overbuilding” the trail/journeyway network, keeping in mind the desire to retain Lantzville’s semi-rural character and to use the community’s limited financial resources wisely.
- c) To identify specific improvements to trails and journeyways, and prioritize them according to their significance to the community and the ability to develop them in the short to medium term. Among these priorities are to develop a trail along the E&N Rail Corridor and to establish a pedestrian link between Upper and Lower Lantzville.

<b>Table 1: Strategies for Trails and Journeyways in Lantzville</b> (from <i>Parks, Trails and Recreation Plan</i> 2008, p.12-13)	
<b>1. Develop and implement a trail and journeyway improvement strategy.</b>	Based on the trail network shown in Map 2, identify and prioritize improvements to trails/ journeyways (or sections thereof) on the basis of: <ul style="list-style-type: none"> <li>• Need - the level of use and prevailing safety concerns;</li> <li>• Opportunities - e.g., upcoming servicing upgrades or development, grants; and</li> <li>• Constraints - e.g., private land, lack of space in existing ROW.</li> </ul>
<b>2. Assist in establishing a trail along the E&amp;N Rail corridor.</b>	Initiate discussions with the Island Corridor Foundation (ICF), the RDN and the Nanoose First Nation to support construction of a trail along the E&N rail corridor.
<b>3. Review and refine standards for trails.</b>	Currently, the Subdivision and Development Bylaw No. 55 sets one standard for pathways (1.5 m width, crush stone surface) and a 2 <sup>nd</sup> standard for trails (2.5 m width, stone or chip surface). These standards should be reviewed to provide for a wider range of trail and pathway situations (e.g., paths through natural areas).
<b>4. Review options and develop a plan for a direct pedestrian connection between Upper Lantzville and the Village core.</b>	Map 2 indicates a potential trail connection, potentially as a pedestrian overpass, across the Island Highway. Past ideas were to make this connection via the Crown property between Harby Road and the Island Highway. However, other locations may be more appropriate.

## 2. Trail and Journeyway Types and General Design Guidelines

*Journeyways* are roads used frequently by pedestrians, cyclists and other forms of nonvehicular transportation<sup>1</sup>. The designation is based on current use by nonvehicular traffic, the capacity to provide access to community destinations (the village centre, beach accesses, parks, trailheads, etc.), and/or the capacity to provide walking, running or cycling loops.

*Trails* are off-road paths used by pedestrians, bicycles and other forms of nonvehicular transportation to connect between neighbourhoods and to destinations within and beyond Lantzville. Trails may be hard (paved, gravel) or soft surfaced (soil/natural) and of varying widths. Existing trails may be on public rights of way, but there are also a variety of informal paths on private property. The intent of this strategy is to ‘formalize’ any trails proposed in this strategy that currently cross private property through agreements with property owners or as part of future development.

Based on the roads and trails designations in Lantzville’s Official Community Plan (Bylaw No. 50.2 2007, Map 6) and the trails and journeyways designations in the Parks, Trails and Recreation Plan (2008 – Map 2), trails and journeyways are shown and classified in **Map A** of this Strategy (attached) as:

- *Major Journeyway*
- *Local Journeyway*
- *Major Trail*
- *Local Trail*
- *Low tide Beach Walk*

The following sections describe each of the trail and journeyway types and suggest general *design guidelines* for them. The guidelines are not intended to provide engineering specifications or standards but rather to indicate a desirable future state for each of these types.

### 2.1 Major Journeyways

Major journeyways are the main routes through the community to and from neighbouring areas. There are five major journeyways identified in Lantzville (Table 2). They are subject to moderate to heavy vehicle traffic, but they are also used heavily by residents

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<sup>1</sup> *Nonvehicular transportation* includes all forms of human-powered transport (walking, jogging and running, cycling, skateboarding, in-line skating, etc.) as well as single-person electrically assisted transportation such as electrically assisted bicycles, carts, scooters and wheelchairs.



and visitors for all modes of nonvehicular transportation. Sufficient room for nonvehicular use and safe separation from vehicles traffic are the primary concerns.

**Design guidelines:** In the short term (1-10 years), the desirable design guideline for all of the major journeyways is a *minimum 1 meter-wide paved shoulder separated by a white line (Figure 1)*.

In the long term (>10 years, as applicable), the desirable design for each major journeyway is described in Table 2.

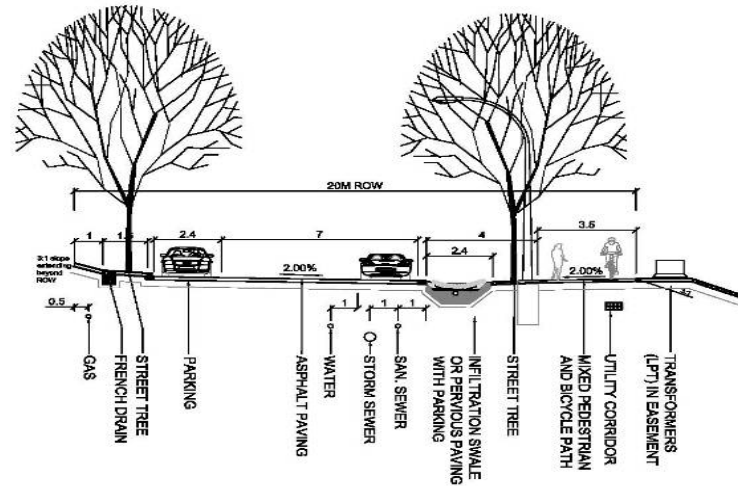
**Table 2: Major Journeyways – Design Guidelines**

<i>Short-term Design for All Major Journeyways</i>	<i>Major Journeyway</i>	<i>Long-term Design</i>
Minimum 1 meter-wide paved shoulder separated by a white line (Figure 1)	Aulds Road	No change once short-term design guideline is met.
	Dickinson Road	<i>Within Village Centre area:</i> 1-meter wide hard surface path separated by boulevard from vehicle lane on one or both sides of road – see Figure 2. This guideline is subject to future Village Centre Plan detail designs for roads and journeyways. Achieving this guideline may be limited by existing trees, power poles and other services within the road right of way (ROW). <i>Remainder of road:</i> no change once short-term design guideline is met.
	Lantzville Road	<i>Within Village Centre area:</i> 1-meter wide hard surface path separated by boulevard from vehicle lane on one or both sides of road – see Figure 2. This guideline is subject to future Village Centre Plan detail designs for roads and journeyways. Achieving this guideline may be limited by existing trees, power poles and other services within the ROW. <i>Remainder of road:</i> no change once short-term design guideline is met.
	Superior Road	No change once short-term design guideline is met.
	Ware Road	<i>From Lantzville Rd to Highway 19:</i> 1-meter wide hard surface path separated by boulevard from vehicle lane on one or both sides of the road – see Figure 2. <i>South of Highway 19:</i> no change once short-term design guideline is met.

Figure 1: Paved shoulder with white line (Lantzville Rd.)



Figure 2: Example of separated walkway within road ROW



## 2.2 Local Journeyways

Local journeyways are secondary roads that provide routes within and between neighbourhoods, often creating local walking, running and cycling 'loops' within the community. They are subject to generally low vehicle traffic, though traffic levels may pick up at peak times (typical pre- and post-working hours) on some neighbourhood collectors (e.g., Petersen Road).

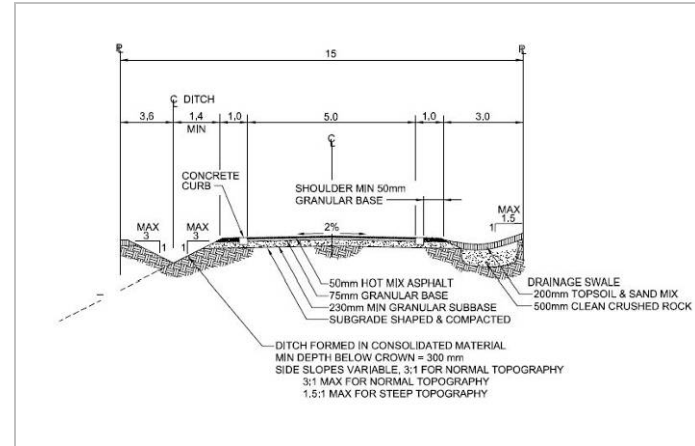
On local journeyways, pedestrians and cyclists share the roadway with vehicles with no physical or marked separation (Figure 3). The roads identified as local journeyways in Map A are considered to provide the most popular local routes. However, local roads that are not identified as local journeyways may still be used by residents but are not expected to meet the following design guideline.

**Design guideline:** There should be sufficient pavement width to allow a vehicle to go around a pedestrian or cyclist without encroaching significantly into the oncoming vehicle lane. Under the District's Engineering Specifications, the minimum pavement width for a local (rural) road is 5 m (Figure 4). Assuming that this is the minimum width needed for two vehicles to pass safely, roads dedicated as local journeyways should eventually have an additional 1 m of pavement - i.e., a minimum total pavement width of 6 m.

Figure 3: Local journeyway example (Southwind Drive)



Figure 4: Rural local road specification from District of Lantzville Bylaw No. 55, page 60



### 2.3 Journeyways - Working in Road Rights of Way

For both major and local journeyways, widening road shoulders, or planning separated pathways within an existing road right of way (ROW), should take the following factors into account:

- The *width of the ROW*, which is the public domain in which public services can be constructed. The width of the ROW is usually greater than the actual paved surface. However, is there sufficient ROW to allow for a public path to be extended or built without trespassing on adjacent private property? In some cases, private uses may encroach into the public ROW. Re-claiming the ROW for public use should be done in a ‘neighbourly’ manner, giving landowners sufficient lead time to adjust to the change.
- The *location of surface and underground services* in the road ROW. Power poles are a major consideration in Lantzville. Narrowing pathways or winding them around poles makes for unsafe conditions for path users; consider which side of a road to build separated pathways to avoid poles. Local engineering standards will define which underground services (water, sewer, natural gas) can lie under a path.
- The *location of trees* within and adjacent to the ROW. Trees are removable, but they also contribute greatly to Lantzville’s rural character and the pedestrian/cycling experience.

## 2.4 Major Trails

Major trails are intended to provide off-road connections through the community to and from neighbouring areas. They would be used by residents and visitors for all modes of nonvehicular transportation.

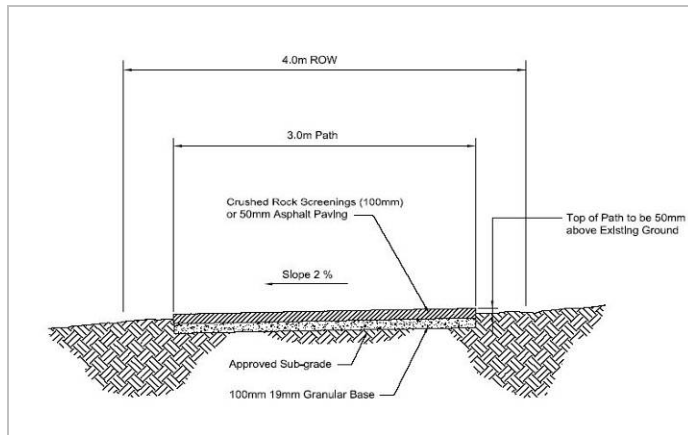
While there are no *existing* major trails in Lantzville, two are proposed for future development in Lantzville (see Map A):

- a major trail running parallel to Ware Road from Lantzville Road to Highway 19; and
- a major trail along the E&N Rail Corridor within the District boundaries.

**Design guideline:** The general design guideline for major trails is a minimum 1 m up to 2.5 m width with a paved or firm gravel surface, in conformance with the multi-use trail specifications in the District’s Subdivision and Development Bylaw No. 55, Drawing No. R1655 (Figure 5). If the major trail is within a road ROW, there would be a minimum 1 meter separation from the vehicle lane.

A future major trail within the E&N Rail Corridor will need to conform to the “Vancouver Island Rail Corridor: Rail-with-Trail Design Guidelines” adopted by the Island Corridor Foundation in June 2009 (Figure 6).

**Figure 5: District of Lantzville Engineering Specification for Multi-Use Trail (Drawing No. R1655, pg. 76, Bylaw No. 55)**



**Figure 6: Example E&N Trail design from “Vancouver Island Rail Corridor: Rail-with-Trail Design Guidelines”, June 2009**

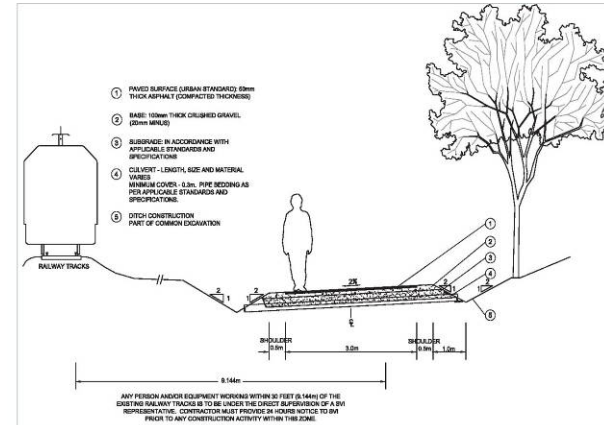


Figure 12. Typical Cross Section B: 30 metre (100 foot) corridor.

## 2.5 Local Trails

Local trails provide off-road connections within and between neighbourhoods in the community. They are used primarily for walking and running, though some may be suitable for bicycles and electric vehicles.

Many local trails in Lantzville have been created over time as a function of informal use rather than design (Figure 7), and some may cross private property. Map A addresses local trails in three situations:

- “Local Trails” that exist now as trails on District or other publicly owned land.
- “Proposed Local Trails on ROW” – future trails on currently undeveloped rights of way (ROW). These are high priorities for trail development as these ROWs are public lands and therefore, need no permissions from property owners, though encroachments by neighbouring landowners may need to be addressed.
- “Possible Trails on Private Property” – these are either :
  - existing informal trails that cross private property that the District would like to ‘formalize’ over time through agreements with the property owners;
  - possible future trails across larger land parcels that should development be proposed in the future, could be acquired as part of future subdivision or development approvals.

**Design guideline:** In the future, designated local trails subject to regular use should *generally* be a minimum 1 meter width, have a reinforced soil, gravel or paved surface, and incorporate drainage management measures (e.g., culverts, boardwalks or footbridges) as needed.

## 2.6 Low Tide Beach Walk

Most of Lantzville’s foreshore can be walked at low tide, with a few exceptions; e.g., the west side of Blunden Point off Sebastion Road where cliffs jut out into the water, and on the east side of Jack’s Road where property infill and riprap extend out into the foreshore preventing passage at most tide levels.

Figure 7: Informal trail at end of Westwind Dr



Photo credit: Lynne Wilson



The beach walk consists of natural beach sands, gravels, cobbles and/or bedrock (Figure 8). No constructed improvements are anticipated.

**Figure 8: Low tide beach walk**



### **2.6.1 Beach Accesses (road ends)**

Section 5.4 of the *Parks, Trails and Recreation Plan (2008)* suggests that the District develop a design plan for waterfront accesses that would lay out a common layout for equipment and facilities, signage, parking and vehicle barriers. The District has since implemented some of these design elements at the Harper Road and Lavender Road accesses. Based on these experiences and the addition of benches and other equipment at other accesses, the District will develop a design guideline for beach accesses over time.

## **2.7 Nature Trails**

*Nature trails* are oriented more towards outdoor recreation than making community connections. They travel through forested or other undeveloped areas and are characterized by a natural soils base. Trails in two areas of Lantzville are of significant interest to the community: Crown Woodlot #1475 and the Foothills Estates lands.

### **2.7.1 Woodlot #1475**

The Copley (Heritage) Trail and associated side trails through Crown Woodlot #1475 (Figure 9) provide an extensive nature trail system. Map A shows the approximate location of the main Copley Trail but there are additional side trails that are not shown.

Protection of the trails in Woodlot #1475 is subject to the approved Woodlot Management Plan and Woodlot Licence Plan. At the same time, the Woodlot Licence Holder is obliged to take community and recreational values associated with these Crown lands into account in generating and executing these Plans.

The Licence Holder for Woodlot #1475 has consulted with the District regarding his plan for the woodlot. *The District intends to continue to work with the Licence Holder to preserve the community's values associated with the Woodlot, and particularly the Copley Trail system. The District would also like to collaborate with the Licence Holder on more accurate trail mapping and coordinating volunteers in the planning and completion of trail improvements and maintenance.*

### 2.7.2 Foothills Trails

Map A also indicates trails in the Foothills Estates property that until recently, were used extensively by area residents. Access to these trails was suspended in late 2009 by the company holding the property due to liability concerns. These trails are based primarily on former logging roads, many of which were upgraded in the last few years in anticipation of upcoming development. It is anticipated that a system of local and nature trails, resembling the current informal network indicated in Map A, will be formalized in time as part of future development of the Foothills property.

## 2.8 Facilities on Trails and Journeyways

### 2.8.1 Benches

The District has a commemorative bench program whereby citizens may purchase a bench to acknowledge or commemorate a loved one (Figure 10). Sites for future benches have been identified within District parks and at public beach accesses. There are potentially useful and attractive sites for benches along District trails and journeyways; these should be identified and added to the sites identified under the District's park bench policy.

Figure 9: Copley Trail



Figure 10: Lantzville bench



### 2.8.2 Bicycle Parking

Facilities to park and lock bicycles (Figure 11) are needed at major destinations such as outside stores and services in the Village Centre and at beach accesses. Inadequate lock-up facilities discourages cycling; haphazard bike parking can be hazardous to pedestrians and motorists. At any given site, it is important to locate these facilities so that they are accessible and safe for cyclists but out of the way of pedestrians and vehicles. Design and color should be chosen to fit into the surroundings while still being theft and vandalism resistant. Note that Development Permit Area V “Village Form and Character” in the Official Community Plan (sec. 11.6.3) specifies that a bicycle rack and bench are to be provided by any new commercial development or redevelopment.

### 2.8.3 Signs

Some of the projects identified in section 3 (below) entail installing directional signs. The District will work towards developing a plan for sign placement based on Map A and a simple and attractive design for trail and journeyway signs, that will help residents and visitors navigate around the trail and journeyway system.

**Figure 11: Bicycle parking examples.** The rack in the lower image is useful where space is limited.





### 3. Trail and Journeyway Projects and Priorities

With input from the Parks and Recreation Commission, 42 improvement projects (Table 3) have been identified for achieving the trails and journeyways network shown in Map A. The following explains the attributes summarized in Table 3 and Map A..

#### 3.1 Project Priorities

The projects are assigned a priority of **1 (high)**, **2 (medium)** or **3 (low)** based on the following factors:

- a) A future trail *within the E&N Rail corridor* is assigned a priority of 1 because of its significance as a major connector within the community and as a potential link to a region-wide trail system. The District of Lantzville partnered with the Regional District of Nanaimo, City of Parksville and Town of Qualicum Beach to examine the feasibility of creating a region-wide trail system within the E&N corridor. This endeavour is supported by the owner of the corridor, the Island Corridor Foundation, provided certain standards and safety considerations are met.
- b) The need to improve public safety due to the current high level of use and potential conflict with vehicle traffic; e.g., improving the road shoulders along Dickinson, Lantzville and Superior Roads are assigned as priority 1 projects.
- c) For *existing* trails and journeyways, projects that are relatively simple and inexpensive to complete are given a high priority; e.g., adding signage or improving trails on public lands..
- d) For *proposed* new trails and journeyways, the presence of an existing public right of way (ROW) is given a high priority. As noted earlier, historic encroachments into undeveloped ROWs may make trail installation more challenging and will require cooperation with neighbouring landowners. Future trails and journeyways proposed where there is no existing ROW are listed as priority 3 and await opportunities to acquire the necessary public access as part of future subdivision or development.

It is important to note that *priorities may change with changing circumstances and opportunities*. Projects currently listed as priority 3 would likely become priority 1 projects should development proposals on the associated lands come forward. In addition, through unforeseen development applications, donations or sale, other opportunities may arise to acquire a trail or journeyway improvement that may be a lower priority, or may not even be on this project list, but which meets the goals of the trails and journeyways strategy. These opportunities should not be foregone.

#### 3.2 Project Descriptors

A *project number* is assigned as a project identifier, to allow the project to be linked to the location shown on Map A. The project number is not intended to reflect an order of priority within each priority category.

The project *location* is a brief description and reference to Map A. Some locations are site specific (e.g., Project 15 refers to installing a sign on Fernmar Road), but the majority refer to the extent of a journeyway or trail or section thereof (e.g., Project 3 refers to the length of Superior Road).

The *type* refers to whether the project involves a major or local journeyway or trail, as described in section 2.

*Neighbourhood* refers to one of five neighbourhoods to assist with indicating distribution of the projects throughout the District:

- Neighbourhood 1: West of Superior Rd – ‘the Winds’ and Bayview Park
- Neighbourhood 2: Superior Road to Ware Road south of Highway 19
- Neighbourhood 3: Superior Road to Ware Road north of Highway 19 (including the Village Centre)
- Neighbourhood 4: East Lantzville - Dickinson & Lantzville Roads
- Neighbourhood 5: Aulds Road area.

### 3.3 Improvements and Cost Considerations

Finally, Table 3 indicates the nature of the *improvements* or actions required and initial *cost considerations* for each project. More detailed design and cost estimates are required for funding applications and for construction.

### 3.4 Funding Sources

Funds and resources to implement the trails and journeyways strategy fall into two general categories: internal sources derived from within the District; and external sources or funding programs.

#### 3.4.1 Internal Sources

- *New development:* As part of new development, the District can acquire the necessary rights of way and negotiate with developers to provide pathways and other pedestrian and bicycle facilities as part of a rezoning application or master development agreement. This method of acquiring future trails and journeyways is noted in many priority 3 projects.
- *Donations and dedications:* In some communities, multi-use pathways have been funded by donors “purchasing” sections of the pathway (e.g., \$100 per meter). Similar to the park bench or tree dedication programs, a program can be set up for residents, corporations and community organizations to donate part of a trail or facilities such as bicycle racks, signs or benches. These donations are tax-deductible where they are administered by a not-for-profit agency.
- *Volunteerism:* Many aspects of trail construction can be accomplished with volunteers, and is an activity that many people enjoy.

- ❑ **Development cost charges:** DCC revenue can be directed towards trail and journeyway improvements where they are included in the applicable DCC bylaw.
- ❑ **Municipal transportation budget:** The District could consider shifting a percentage of its current budget for road infrastructure to support active transportation improvements. This is increasingly being done by municipal governments that want to see real change in their modal split. For example, Edmonton’s City Council recently endorsed 1.5% of its transportation department’s capital budget to be directed to “active transportation” projects (walkways, bike lanes, etc.), with a recommendation to increase this to 5% in the next 10 budget plan.

### 3.4.2 External Sources

It is important to note that many public funding programs, such as those described below, are set up on a cost-sharing basis with the grant applicant. Also, to take advantage of many of these programs requires that the District complete detailed designs and accurate cost estimates. The costs of preparing detailed designs are typically not eligible for cost share funding — only the capital costs of construction are eligible.

- ❑ **BikeBC** is a recently–announced program administered by the Ministry of Transportation and Infrastructure that provides a total of \$31 million for bicycle-related infrastructure throughout BC. The program is a strategic investment to build important cycling corridors of regional and provincial significance and will be awarded province wide over the next three years. Local Governments may apply to Bike BC through the Ministry’s Cycling Infrastructure Partnerships Program or contact the ministry at [MoTcycling@gov.bc.ca](mailto:MoTcycling@gov.bc.ca).
- ❑ **Cycling Infrastructure Partnerships Program (CIPP).** Through this program, the Ministry of Transportation and Infrastructure provides up to 50% cost-sharing (to a maximum of \$250,000 per project) for new and improved bicycle facilities on secondary highways and classified arterials. Under the roads section of the Provincial Revenue Share Act, grants are awarded to assist in the development of major municipal roads, and bicycle facilities are eligible under this program.
- ❑ **LiveSmart BC.** The LiveSmart BC Green Cities Awards is a provincial program aimed at rewarding leading edge communities for initiatives aimed at making them greener and healthier. Financial awards vary by population size, ranging from \$25,000 to \$100,000.
- ❑ **Infrastructure Canada** manages several programs which provide funding for environmental and local transportation infrastructure projects in municipalities across Canada. Typically, the Federal government contributes one-third of the cost of municipal infrastructure projects. Provincial and municipal governments contribute the remaining funds, and in some instances, there may be private sector investment as well.

- ❑ **Green Municipal Funds.** The Federation of Canadian Municipalities manages the Green Municipal Fund, with a total allocation of \$550 million. This fund is intended to support municipal government efforts to reduce pollution, reduce greenhouse gas emissions and improve quality of life. The expectation is that knowledge and experience gained in best practices and innovative environmental projects will be applied to national infrastructure projects.
- ❑ **Rural Infrastructure Fund.** The Canada/BC Municipal Rural Infrastructure Fund is a grant program for infrastructure in communities with populations less than 250,000. Its purpose is improving municipal and rural infrastructure to ensure that communities are sustainable, competitive and healthy centres of economic growth.
- ❑ **EcoMOBILITY** is an initiative administered by Transport Canada under the Canadian ecoACTION program aiming at reducing passenger transportation emissions by promoting less polluting forms of transportation, such as walking, cycling, public transit and ridesharing.
- ❑ **Other federal programs.** At any given time, there are usually one or more Federal grant programs for which trails and trail facilities would be eligible. As an example, in the past, Environment Canada provided grants through the Environmental Partners Fund for bicycle-related projects which demonstrated a benefit to the environment and which formed partnerships with the community. It is important to note that eligibility for some Federal programs is limited to not-for-profit organizations. By forming partnerships with local not-for-profit organizations, the District can access a number of alternative funding sources and grant programs for bicycle projects. It is important to note that because the primary applicant for funds is the not-for-profit group, they are nominally in charge of the project.
- ❑ **ICBC** has in the past provided funding for bicycle facilities, particularly where these have the potential to reduce crashes and claims costs to ICBC. Check with ICBC's Road Improvement Program and Safer City Program.

**Table 3: LANTZVILLE TRAILS & JOURNEYWAYS - IMPROVEMENT PROJECTS AND THEIR PRIORITIES**

Project #	Priority (1 - 3, see text)	Location	Type: 1-J Major J'way 2-J Local J'way 1-T Major Trail 2-T Local Trail	Improvements	Cost Considerations Unit costs are based on typical 2009 cost estimates; they are suitable for general budgeting only and are accurate to +/- 30% (Class D estimate) unless specified otherwise.
1	1	Dickinson Rd - District boundary to Lantzville Rd intersection	1 - J	There is an existing white-line marked shoulder walkway, but many sections may not be 1 m wide. Assess for sufficient width and add pavement or realign white line where needed.	Approx. \$163/l.m. for added shoulder walkway (1 m width)
2	1	Lantzville Rd (east) – Dickinson Rd east to District boundary	1 - J	There is an existing white-line marked shoulder walkway, but some portions may not be 1 m wide. Assess for sufficient width and add pavement or realign white line where needed.	Approx. \$163/l.m. for added shoulder walkway (1 m width)
3	1	Superior Rd	1 - J	Assess pavement width and shoulder condition, add white line marking for 1-m wide shoulder walkway on 1 side (realign centre line if necessary).	Approx. \$163/l.m. for added shoulder walkway (1 m width). \$5/l.m. for line marking only
4	1	E&N Corridor Trail – Aulds to Ware Rd	1 - T	Gain approval of Island Corridor Foundation; develop design as per “Vancouver Island Rail Corridor Rail-with-Trail Design Guidelines” (June 2009); apply for grants; construct.	Construction - \$500,417 +/- 25%* *Based on “Rail with Trail Feasibility Study – RDN”, April 2009. Cost estimate includes: 5-6 m wide clearing, sub-grade preparation to maximum 5% grade, cross-culverts and ditches where required, 4 m of -3/8” crushed gravel surface compacted to 6” depth. Does NOT include costs of: surveying, engineered design, project management, bridges if needed, paving if desired.
5	1	E&N Corridor Trail – Ware to Superior Rd	1 - T	Same as above.	Construction - \$638,864 +/- 25%* * see above
6	1	Lucas Lane, between Lavender and Myron Rd	2 - J	Proposed future trail in existing ROW; assess encroachments, discuss with neighbouring property owners, complete trail to connect both ends of the lane.	Approx. \$33 per lin meter for 2-m wide gravel surface or \$20/l.m. for 1-m wide gravel surface
7	1	Jenna Dr	2 - T	Proposed future trail in existing narrow ROW from Jenna Dr to Crown parcel (see project #24 for continuation to Dickinson Rd).	Approx. \$33 per lin meter for 2-m wide gravel surface or \$20/l.m. for 1-m wide gravel surface

Project #	Priority (1 - 3, see text)	Location	Type: 1-J Major J'way 2-J Local J'way 1-T Major Trail 2-T Local Trail	Improvements	Cost Considerations Unit costs are based on typical 2009 cost estimates; they are suitable for general budgeting only and are accurate to +/- 30% (Class D estimate) unless specified otherwise.
8	1	Owen Rd (east end) to Dickinson Rd	2 - T	Existing trail in ROW; assess state of trail and encroachments, improve and sign the trail.	Approx. \$33 per lin meter for 2-m wide gravel surface or \$20/l.m. for 1-m wide gravel surface
9	1	Saxon Cross trail to Dickinson Rd	2 - T	Existing well-used trail: add signs at either end to indicate public access; assess trail condition and improve if needed.	Typical sign cost - \$50.00?
10	1	Rossiter Ave to Lynn Dr	2 - T	Proposed future trail in existing (narrow) ROW. Assess encroachments, discuss with neighboring property owners, design and build.	Approx. \$33 per lin meter for 2-m wide gravel surface or \$20/l.m. for 1-m wide gravel surface
11	1	Two ROWs off north side of Lantzville Rd to waterfront bluff	2 - T	Assess which ROW would be most appropriate from public safety and enjoyment perspectives; design trail and viewpoint.	Approx. \$33 per lin meter for 2-m wide gravel surface or \$20/l.m. for 1-m wide gravel surface.
12	1	Harwood Rd to Phillips Rd connector	2 - T	Proposed future trail in existing narrow ROW. Assess encroachments, discuss with neighboring property owners, design and build trail.	Approx. \$33 per lin meter for 2-m wide gravel surface or \$20/l.m. for 1-m wide gravel surface
13	3	Harwood Rd (west end) to Harby W Rd	2 - T	Possible future trail on private property: initiate discussions with owner of large parcel regarding a possible trail along east boundary of property.	
14	1	Fernmar (west end) south to powerline and east to Aulds Rd	2 - T	Proposed future trails in existing ROWs; design and build trail. In collaboration with the Woodlot Licence holder, install a sign to Copley Trail.	Approx. \$33 per lin meter for 2-m wide gravel surface or \$20/l.m. for 1-m wide gravel surface
15	1	Fernmar Rd (mid) to Andrea Cres via Copley Park	2 - T	Existing trail to Copley Park: add a public access/park sign on Fernmar pointing to Copley Park	Typical sign cost - \$50.00?
16	1	Romac Rd – between Lorenzen Lane and Northwind Dr	2 - T	Proposed future trail in existing Romac Rd ROW: road has not been developed. Assess encroachments and discuss with neighboring property owners, design and build trail.	Approx. \$33 per lin meter for 2-m wide gravel surface or \$20/l.m. for 1-m wide gravel surface
17	1	North Rd beach access	2 - T	Proposed future trail in existing North Rd ROW: determine best route, design and build trail along existing ROW	Approx. \$33 per lin meter for 2-m wide gravel surface or \$20/l.m. for 1-m wide gravel surface

Project #	Priority (1 - 3, see text)	Location	Type: 1-J Major J'way 2-J Local J'way 1-T Major Trail 2-T Local Trail	Improvements	Cost Considerations Unit costs are based on typical 2009 cost estimates; they are suitable for general budgeting only and are accurate to +/- 30% (Class D estimate) unless specified otherwise.
18	2	Lantzville Rd (central) - between Superior & Ware Roads	1 - J	Existing white-line separated paved shoulder; assess for sufficient width; add pavement or realign white line where needed. Plan for a separated pathway (major trail) in a future Village Centre design, approximately from Huddleston to Ware Rd.	Approx. \$163/l.m. for added shoulder walkway (1 m width). \$5/l.m. for line marking only.
19	1	Ware Rd - Hwy 19 south to Aulds and Harwood	1 - J	The white-line separated shoulder walkway extends only as far as Industrial Rd/Clark Drive W. Assess pavement width and shoulder condition, add white line separation for 1-m wide walkway on 1 side at least (realign centre line if necessary)	Approx. \$163/l.m. for added shoulder walkway (1 m width). \$5/l.m. for line marking only
20	2	Lantzville Rd (west) - from Superior Rd west to junction with Hwy 19	1 - J	Assess pavement width and shoulder condition; add white line marking for 1-m wide shoulder walkway on at least 1 side (realign centre line if necessary).	Approx. \$163/l.m. for added shoulder walkway (1 m width). \$5/l.m. for line marking only
21	2	Highway 19 footbridge crossing	1 - T	Considered in vicinity of Phillips Rd (south side) and Harby E Rd (north side of Highway). Examine plausible locations, approach MOT, research funding sources.	
22	2	Southwind Dr	2 - J	Assess road pavement width (approx 7.5 m) and shoulder condition, add white line marking for 1-m wide walk-bikeway on 1 side (realign centre line if necessary).	Approx. \$163/l.m. for added shoulder walkway (1 m width). \$5/l.m. for line marking only
23	2	Stronge Rd - between Jack's and Lantzville Rd	2 - T	Proposed future trail in existing ROW; however, a crossing of Bloods Creek is involved in linking Jacks to Stronge Rd. Assess feasibility and type of bridge required to meet fisheries/riparian area requirements.	
24	2	Jenna Dr to Dickinson Rd	2 - T	Possible future trail: follow the north (lower) end of Bloods Creek through on provincial Crown parcel. Apply to the Province for Free Crown Grant, Nominal Rent Tenure, easement for public access, or other means of acquiring title or rights.	Staff time.

Project #	Priority (1 - 3, see text)	Location	Type: 1-J Major J'way 2-J Local J'way 1-T Major Trail 2-T Local Trail	Improvements	Cost Considerations Unit costs are based on typical 2009 cost estimates; they are suitable for general budgeting only and are accurate to +/- 30% (Class D estimate) unless specified otherwise.
25	2	South end of Petersen Rd to corner Hwy 19 and Ware	2 - T	Partly on MOT land and partly on District property. Negotiate with MOT for access; route and build trail.	Approx. \$33 per lin meter for 2-m wide gravel surface or \$20/l.m. for 1-m wide gravel surface
26	1	Fernmar (west end) north to Harby W Rd	2 - T	Proposed future trail on existing ROW that runs through private golf course. Initiate discussions with golf course and neighboring property owners for future trail access on ROW.	
27	2	Legacy Marsh, southeast end	2 - T	Possible future trail on private property: approach landowner and BC Nature Trust to consider short trail and viewing platform (latter to be constructed on District property at east end?).	Approx. \$33 per lin meter for 2-m wide gravel surface or \$20/l.m. for 1-m wide gravel surface. \$3825/l.m. for 2m wide boardwalk with metal handrail. \$1470/sq.m. for lookout or platform
28	2	Clark Dr - connection between east and west portions	2 - T	Possible future trail and/or journeyway: acquire ROW to link east and west portions of Clark Drive; build a trail in advance of road.	Approx. \$33 per lin meter for 2-m wide gravel surface or \$20/l.m. for 1-m wide gravel surface
29	2	Lynn Rd to Schoolhouse Dr	2-T	Existing, heavily-used informal trail; verify property ownership and gain agreement of landowner for public use. In long term, upgrade trail.	
30	3	Ware Rd - Hwy 19 north to Lantzville Rd	1 - T	Acquire separated multi-use trail along Ware Rd as part of future development of parcel.	
31	3	Bloods Cr trail between Dickinson and Lantzville Roads	2 - T	Acquire extension of Jenna Rd trail to Dickinson Road if parcel to east of Crown land redevelops or subdivides in future	
32	3	Lantzville-Schook intersection to Lantzville Rd	2 - T	Possible future trail: an existing ROW runs off the north side of Lantzville Rd to the NW corner of the parcel at the corner of Schook and Lantzville Roads. Acquire an extension of the ROW and complete a trail to Schook Rd. if/when the parcel develops or subdivides in future.	



Project #	Priority (1 - 3, see text)	Location	Type: 1-J Major J'way 2-J Local J'way 1-T Major Trail 2-T Local Trail	Improvements	Cost Considerations Unit costs are based on typical 2009 cost estimates; they are suitable for general budgeting only and are accurate to +/- 30% (Class D estimate) unless specified otherwise.
33	3	Wiles Rd to Ware Rd	2 - T	Acquire ROWs/trails if parcels redevelop or subdivide in future.	
34	3	Lantzville Properties (west side of Ware Rd N)	2 - T	Acquire ROWs/trails as part of future development of this parcel.	
35	3	Lynn Rd to Lantzville Rd	2 - T	Acquire ROWs/trails as part of future development of these parcels.	
36	3	Sebastion west to Sunbury Rd	2 - T	Partial existing ROW ("Cal Rd."); acquire remaining ROW if associated parcels redevelop or subdivide.	
37	3	Hobsons to Stone Rd	2 - T	Acquire ROW and trail as part of future development of parcel.	
38	3	North end Normarel to E&N corridor	2 - T	Acquire ROW and trail as part of future development of parcel.	
39	3	Southwind-Bayview connector	2 - T	Acquire ROW and trail as part of future development of parcels.	
40	3	Ware Rd to Hase Place and Elm Rd	2 - T	Acquire ROW and trail if parcels develop or subdivide in future	
41	3	Ware/Aulds Rd corner to Phantom Rd	2 - T	Acquire ROW and trail if parcel redevelops or subdivides in future	
42	3	Westwind Rd connector to Lorenzen Rd	2 - T	Acquire ROW and trail if parcel redevelops or subdivides in future	

**Map A  
Lantzville Trails &  
Journeyways Strategy  
- Plan\*** \*For planning purposes only

November 2010

**Legend**

- Major Road/Journeyway
- Local Road/Journeyway
- Proposed Local Road/Journeyway
- Proposed Major Trail
- Local Trail
- Proposed Local Trail on ROW\*
- Possible Trail on Private Property\*\*

\* Currently not trails; shown for long term planning purposes only. \*\* Subject to landowner agreement; see Sec. 2.5

- Beach Low Tide Walk
- Potential Woodlot Trails \*\*
- Potential Foothills Trails \*\*

\*\* Trails are shown for illustrative purposes only, and are not accurate with respect to location and extent; there may be more trails than those shown. See Sec. 2.7

**Projects**

Note: Numbers in circles correspond to Projects listed in Table 3. Priorities may change as opportunities arise; see Sec. 3.1

- First Priority
- Second Priority
- Third Priority

**Landmarks**

- Fire Hall
- Municipal Hall
- School
- Proposed Park
- Existing Viewpoint
- Proposed Viewpoint
- Lantzville Boundary
- Municipal Parks
- Woodlot
- Agricultural Land Reserve

